

Chapter 4

PLACES

North Glendale is a unique collection of varied neighborhoods and districts. It includes commercial and residential places, such as Montrose Shopping Park, Sparr Heights and Highway Highlands. Each has a unique development pattern created during various time periods. The North Glendale Community Plan seeks to preserve and enhance each area's distinct character, while identifying the vision for each area and potential improvement.

4.0 Introduction

While Chapter 2: Community Vision and Chapter 3: Principles provide an overall planning context for the Community Plan area, it is in this Chapter 4: Places where each neighborhood and district in North Glendale is described. This Chapter groups the neighborhoods together into seven major areas based on similar characteristics, or a common identity as understood by the community. For example, the Glenwood Oaks, Mountain Oaks, Whiting Woods, and Oakmont Woods neighborhoods are grouped together because they are single-family residential neighborhoods located on the northeast face of the Verdugo Mountains, with like climate and vegetation. Commercial and residential areas are combined where they are seen to function together as a whole, such as the Montrose and Sparr Heights neighborhoods.

The seven areas are:

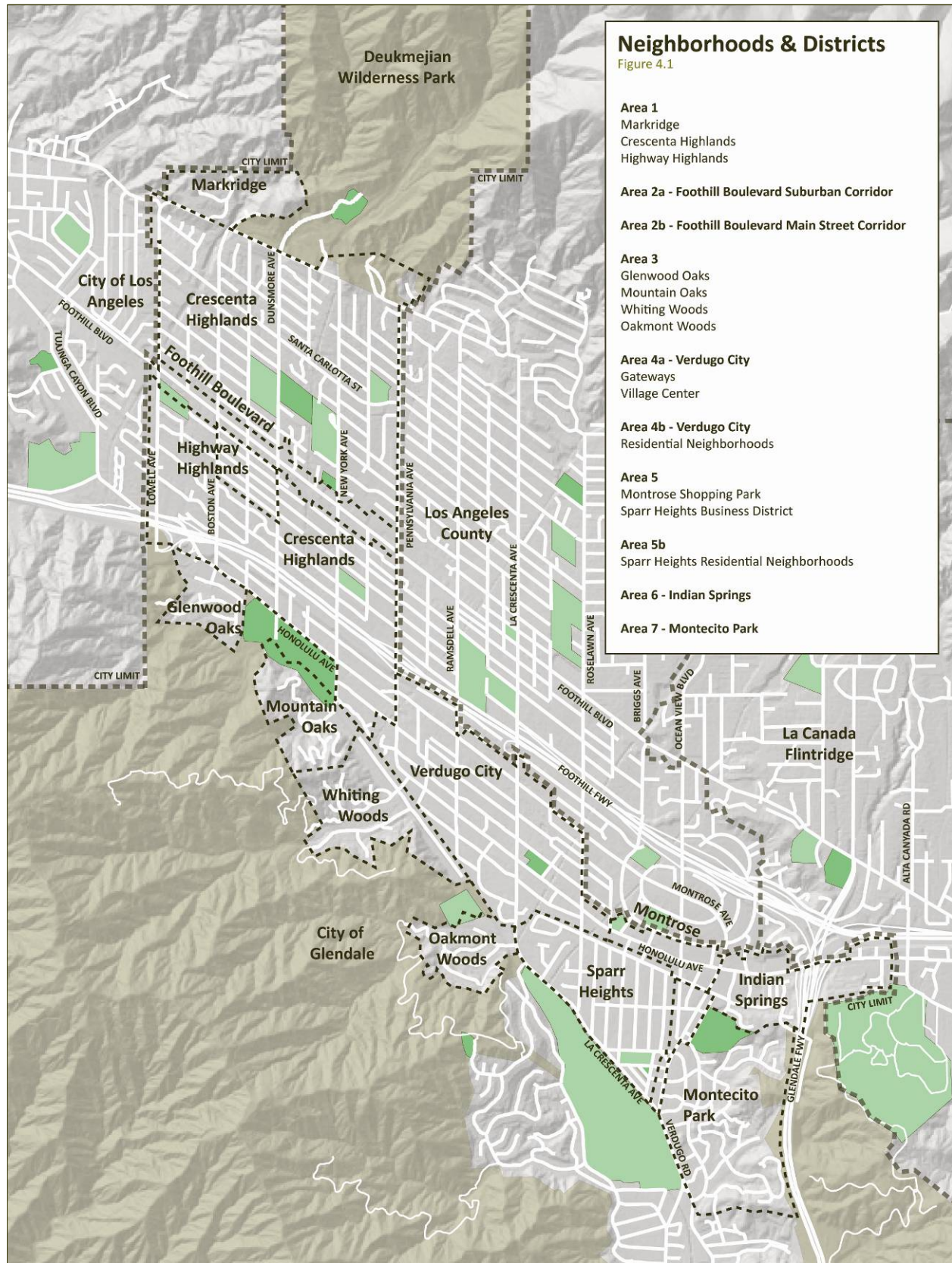
- Area 1 - Markridge, Crescenta Highlands, Highway Highlands Residential Neighborhoods
- Area 2 - Foothill Boulevard, described in two sections:
 - A – “Suburban Corridor” Commercial District
 - B – “Main Street” Commercial District
- Area 3 - Verdugo City, described in two sections:
 - A – “Gateways” and “Village Center” Commercial Districts
 - B – Residential Neighborhoods
- Area 4 - Glenwood Oaks, Mountain Oaks, Whiting Woods, and Oakmont Woods Residential Neighborhoods
- Area 5 - Montrose and Sparr Heights, described in two sections:
 - A – Montrose Shopping Park and Sparr Heights Business Districts
 - B – Sparr Heights Residential Neighborhoods
- Area 6 - Indian Springs Commercial and Residential District
- Area 7 - Montecito Park Residential Neighborhood

Each area includes the following sections to guide future development:

A **Vision** for the future based on existing characteristics together with community desires and expectations. The Vision is intended to describe the growth and land use policies outlined in Chapter 3.

Public Improvements to the street system, transit network, bikeways, utilities, and parks that will assist in implementing the Vision.

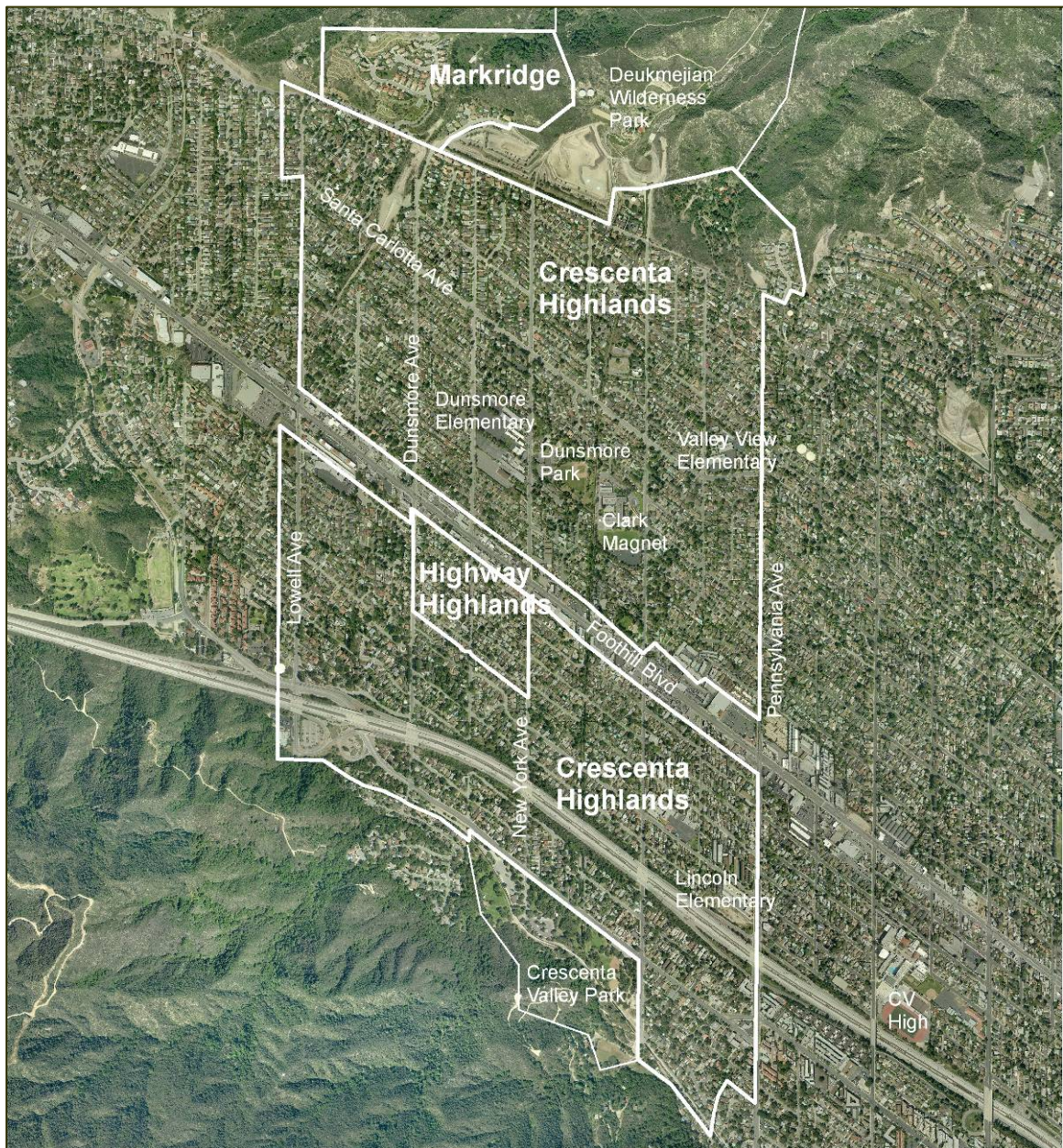
Design Guidelines specific to each area, necessary to help implement the Vision. It is intended that the design guidelines of Chapter 4 will be implemented for every proposed addition or new building. The design guidelines are intended to convey overall best practices. However, conditions vary from site to site, and there may be a more appropriate solution that is not included in the guidelines. Innovative design solutions consistent with the spirit of the neighborhood vision will be considered and even encouraged. As needed, these guidelines can be supplemented by the applicable section(s) of the Comprehensive Design Guidelines.



4.1 Markridge, Crescenta Highlands and Highway Highlands

4.1.1 Vision

These three single-family residential neighborhoods are located in the northernmost portion of Glendale, mostly between the City boundaries of Lowell Avenue to the west and Pennsylvania Avenue to the east. They are grouped together because of their low density and location. Although these neighborhoods share many similarities, they were developed during different time periods and each has unique characteristics that will be maintained. New homes or additions should be sensitive to the overall scale of the neighborhood and incorporate natural materials, but are not expected to mirror existing designs. These areas also share access to open spaces, including Deukmejian Wilderness Park.



Markridge is located on the south face of the San Gabriel Mountains with valley views. This 42-home hillside subdivision is designated by entry signs at Markridge Road and is designed around “restricted use” areas that follow the Sierra Madre Fault. The neighborhood features flat lots cut into the hillside with two-story homes of similar design that are generally larger than others in North Glendale. The steep streets with curbs, sidewalks (but no parkway), street trees located at the edge of the sidewalk, and tennis courts will be maintained. Parkways may be added with community input.. New houses, additions or remodels should fit into the neighborhood but are not expected to copy existing designs.



Houses in Markridge have similar one- or two- story porticos and front-facing 3-car garages

Crescenta Highlands neighborhoods feature an eclectic mix of design styles. Setbacks, massing, garage location, sidewalks, curbs, street lights and utilities vary from street to street. Expansion of existing homes and new homes should be sensitive to the overall scale of the surrounding context and maintain the mix of styles that gives the area its charm. Many streets have a rural character, and the construction of sidewalks and street lights should be done when desired by residents.



Eclectic collection of buildings with stone walls is characteristic of the Crescenta Highlands

Highway Highlands is a distinct neighborhood set within the larger Crescenta Highlands neighborhood. The angled street grid is unique, with streets that typically do not feature sidewalks or mid-block street lights. This neighborhood contains a concentration of historic stone houses built in the 1920s, along with considerable lengths of stone retaining walls at the front of many upsloping properties. This charming variety of architectural styles and the pattern of mostly small-scale homes on small parcels should be maintained within this neighborhood.



Street facing houses with open front yards are typical in Crescenta Highlands



Small lots with stone retaining walls are characteristic of Highway Highlands

4.1.2 Public Improvements

Many streets in these neighborhoods do not have sidewalks, parkways, lighting and/or curbs. These improvements should be made only when interest is clearly expressed by the residents. This area is characterized by drainage channels which bisect the neighborhoods.

4.1.3 Design Guidelines

In addition to the policies noted in Chapter 3, and the Vision for the future listed above, the following design guidelines are specific to Markridge, Crescenta Highlands and Highway Highlands neighborhoods. As needed, these may be supplemented by the Comprehensive Design Guidelines. The design guidelines are intended to convey overall best practices. However, conditions vary from site to site, and there may be a more appropriate solution that is not included in the guidelines. *Innovative design solutions that are consistent with the spirit of the community vision will be considered and even encouraged.*

Site Planning involves a careful analysis of the opportunities and constraints of the site, including existing features such as mature trees, topography, and drainage patterns. The components of site development extend beyond building placement and configuration, including surrounding uses, retaining walls, landscape design, hardscape considerations, and parking.

A. Building Location

1. When there is a clearly distinguishable neighborhood pattern, new development should respect the established pattern with building location, pedestrian and vehicular access. When the development pattern varies, new development should be designed to fit into the immediate context.



Open front yards with mature landscaping are common in Crescenta Highlands.



Curvilinear streets with sidewalks and open front yards characterize Markridge.

B. Yards and Usable Open Space

1. Front yards in this area are open to the street, with the exception of some sloping sites which require retaining walls.

C. Garage Location and Driveways

1. Much existing development has parking and garages directly facing the street. However, the garage may face away from the street, toward the interior lot line.

D. Landscape Design (Including Hardscape)

1. Large mature trees are often characteristic of these properties. Existing trees should be maintained. New landscape design should include canopy trees of substantial size to maintain this characteristic. In areas with few trees, native and California-friendly trees should be considered.

E. Walls and Fences

1. The front yard should maintain its open appearance toward the street. Fences may be appropriate, where permitted. Design, materials and details must meet design and zoning criteria.

F. Retaining Walls

1. Further use of retaining walls to create level front yards should be minimized, and when used cobble or native rock should be used where possible. Other natural materials may be used if necessary.

Mass and Scale – New projects should fit well with surrounding building fabric. While new projects need not copy existing development, mass and scale should respect adjacent building context.

G. Relate Buildings to Existing Context

1. Much of the existing development in Crescenta Highlands and Highway Highlands is single story. Second stories in these neighborhoods should be sensitive to the existing context.
2. Consider setting back a new second story from the front face of the ground floor.

H. Relate Buildings to Existing Topography

1. Sites in this area slope in one or two directions. Design should be sensitive to hillside topography and lot configuration. Locate the taller portions of the buildings on the upslope portion of the site so the building profile reflects the topography in one or both directions.

I. Scale, Proportion and Monumentality

1. These neighborhoods currently consist of homes that appear relatively modest as viewed from the street, regardless of their size. Larger homes should respect the existing context and not appear monumental, especially as viewed from the street.



Modest homes on small lots and block wall retaining walls are a typical on up-sloping lots in Highway Highlands.



Designs should be sensitive to hillside topography and lot configuration.

J. Roof Forms

1. Existing roof forms are varied and most are pitched roofs. Due to the variety of styles, there are no preferred roof forms. Flat roofs are not inappropriate and should be designed with consideration for the existing context.

Design and Detailing of buildings is paramount to a quality environment. Detailing and choice of materials should reinforce the overall project design. Architectural design elements, details and materials should be consistent throughout a project, recognizing that a building is 3-dimensional and must be well designed on all sides.

K. Overall Design and Detailing

1. Existing residences are well crafted, typically well detailed with two or three high quality materials. New structures DO NOT need to match the design or style of existing structures, but should be equally well crafted.

L. Entryways

1. Entries should be well integrated into the overall design, open to and visible from the street, but not monumental in scale or character.

M. Finish Materials

1. Natural and local materials should be used wherever possible.
2. Stone walls and stone houses characteristic of this area should be retained.

N. Wall Thickness

1. For traditional architectural designs, exterior walls should be thick enough to provide a window recess.



Natural stone and local materials should be used wherever possible



Stone walls well-crafted windows and a variety of materials provide texture and detail characteristic of Crescenta Highlands

4.2a Foothill Boulevard “Suburban Corridor”

4.2a.1 Vision

Foothill Boulevard provides a continuous link from the San Fernando Valley to Pasadena as it runs through the four jurisdictions of the Crescenta Valley (Tujunga in the City of Los Angeles, City of Glendale, Los Angeles County and City of La Canada Flintridge). It is the spine or backbone that ties the Valley community together. The majority of Foothill Boulevard is a typical “Suburban Corridor” characterized by land uses and development oriented towards automobile access. However, building design, landscaping and streetscape should be enhanced to provide a more visually pleasant experience for all users, creating an environment where existing and new businesses thrive. This suburban corridor, with a variety of uses, should be attractive and vibrant in the future.





Foothill Boulevard Suburban Corridor

The **Foothill Boulevard Suburban Corridor** accommodates a variety of uses, building heights, architectural styles, and site configurations and is characterized by larger lots and auto-oriented development. Building setbacks and parking lot locations vary from lot to lot, and there is no consistent street edge. Commercial activity will continue to be neighborhood and community service oriented, including automotive sales and repair. The street should continue to have an eclectic feel with variations in building location, style, parking location and open space.

Streetscape improvements should have a consistent pattern to increase visual appeal and pedestrian comfort. Complete street principles should be used to accommodate all users, from pedestrians to cyclists, buses, automobiles, and delivery trucks. Such improvements must also address safety features including new crossings and improved drainage. This new streetscape will create a business district even more successful than today's.



Building set back from street with landscaping and outdoor dining



Landscape buffer creates welcoming pedestrian environment



Landscaping, street furniture, trees and wide sidewalks provide a comfortable pedestrian environment

4.2a.2 Public Improvements

The Foothill Boulevard Suburban Corridor is a prime example of a “Complete Street.” While this corridor is part of the Primary Auto Network as an alternate to the 210 Freeway, it is also a Primary Bikeway connecting through the Crescenta Valley, as well as a Primary Transit Street with Metro bus service connecting to the Northern San Fernando Valley and Glendale. Mobility improvements on Foothill Boulevard aim to increase mobility, access, and safety for all users while maintaining the existing number of auto travel lanes, minimizing the removal of street parking and maintaining the center turn lane. However, the street section may need transformation to provide the necessary improvements.

A. Streetscape, landscape and intersection improvements for all users

1. Consider the following enhancements within the right-of-way: (a) curb extensions with parkway landscaping and widened sidewalks, (b) landscaping with drainage swales, (c) dedicated travel lanes for buses, (d) limited, raised landscape center medians at entry points to Glendale.
2. Provide adequate ADA clearance for all street and mobility infrastructure improvements.
3. Coordinate street tree planting, sidewalk widening and replacement trees with any future development site or street improvement projects along Foothill Boulevard.
4. Consider coordinating and connecting infrastructure improvements associated with the Safe Routes to School Program to Foothill Boulevard, including improvements at Dunsmore and Valley View Elementary Schools.
5. Support a signalized pedestrian crossing on the Foothill Boulevard and Maryland Avenue intersection adjacent to the Post Office and other areas as deemed necessary by future traffic studies.



Enhanced crosswalks at intersections along Foothill Boulevard increase visibility and safety for pedestrians.



Curb extensions at crosswalks are supported along Foothill Boulevard to increase safety and enhance mobility for all users.



Curb extensions at midblock locations can be used to increase parkway landscape and provide wider sidewalks with benches and other pedestrian amenities.

B. Improvements to the Primary Bikeway Network

1. Maintain the existing Class 2 dedicated bike lane on Foothill Boulevard.
2. Provide conveniently located bicycle parking.
3. Support enhanced bicycle infrastructure as supported in Chapter 5 of the Safe and Healthy Streets Plan on streets identified as Primary Bikeways, including Foothill Boulevard and New York Avenue.

C. Improvements to the Primary Transit Street Network

1. Maintain existing transit service on Foothill Boulevard; consider opportunities for providing additional service when feasible.
2. Consider relocating all bus stops after intersections or provide queue jumps for bus stops located before intersections for enhanced mobility.
3. Enhance bus stop amenities where possible, including shelters and/or shaded waiting areas, landscaping, benches and trash cans.

D. Improvements to Parking Management and Resources

1. Consider lifting existing time restrictions for on-street parking.

E. Public Utilities

1. Pursue funding sources, excluding assessment districts, to underground overhead utilities.
2. Provide drainage improvements in cooperation with the Los Angeles Dept of Public Works and NPDES.
3. Support use of purple pipe water for landscaping where available.



Maintaining the existing Class 2 dedicated bicycle lane along Foothill Boulevard is supported, as it is an important link in the region’s bicycle network.



Bus stops along Foothill Boulevard should include amenities such as shelters, trash cans and benches.

4.2a.3 Design Guidelines

In addition to the policies noted in Chapter 3, and the Vision for the future listed above, the following design guidelines are specific to the Suburban Corridor portion of Foothill Boulevard. As needed, these may be supplemented by the Comprehensive Design Guidelines. The design guidelines are intended to convey overall best practices. However, conditions vary from site to site, and there may be a more appropriate solution that is not included in the guidelines. *Innovative design solutions that are consistent with the spirit of the community vision will be considered and even encouraged.*

Site Planning involves a careful analysis of the opportunities and constraints of the site, including existing features such as mature trees, topography, and drainage patterns. The components of site development extend beyond building placement and configuration, including surrounding uses, retaining walls, landscape design, hardscape considerations, and parking.

A. Building Location

1. In the Suburban Corridor portion of Foothill Boulevard, buildings may be located near the front of the lot with parking behind, or toward the back of the lot with the parking in front.
2. Regardless of building location, the ground floor facing the street should have an open appearance.
3. If the proposal is for a corner site, the building should be located at or near the corner.
4. Coordinate building improvements with trees, bus stops and other elements in the public right-of-way.



If the proposal is for a corner site, buildings should be located at or near corner.



Building set back with sidewalk dining provides and inviting streetscape.

B. Usable Open Spaces

1. Consider exterior open space at or near the street to enhance the pedestrian experience, but allow flexibility for open space for new development.
2. Well-planned, exterior open space with well-designed landscaping is important to the overall project design.



Sidewalk landscaping creates a welcoming pedestrian environment



Landscape to screen parking from the sidewalk is encouraged for all new development sites along Foothill Boulevard.

C. Parking

1. Parking may be in front of or behind the building.
2. Use of permeable paving, swales, and other techniques for storm water percolations is highly encouraged.

D. Landscaping

1. Landscaping should be provided near sidewalks to improve the pedestrian experience.
2. Provide a landscape buffer between sidewalks and surface parking.
3. Provide sufficient landscaping between commercial development and adjacent residential zones.

E. Retaining Walls

1. Provide for stepped retaining walls and/or minimize the use of retaining walls to alter grades.



Retaining walls should be terraced with landscaping between.



Retaining walls should be terraced with landscaping between.

Mass and Scale – New projects should fit well with surrounding building fabric. While new projects need not copy existing development, mass and scale should respect adjacent building context.

F. Relate Buildings to Existing Context

1. As new development is often larger in size and mass than existing neighboring structures, a building may need to be expressed as a series of separate volumes.
2. To provide massing to fit with surrounding residences and a human scale, long, continuous segments of building walls facing the public street should be avoided.
3. New development should reflect the existing development pattern.
4. Buildings with greater than 100 linear feet of frontage should include significant breaks and/or sufficient architectural interest to reflect existing development pattern.
5. Building massing should assist in providing effective transitions between commercial and residential zones.



Buildings can be set close to the street. A variety of building sizes and forms that relate well and visually appealing streetscape.



Building roof lines and massing should be terraced to echo the topography.

G. Relate Buildings to Existing Topography

1. Building heights should follow existing topography.

H. Roof Forms

1. Encourage buildings with varying roof heights, allowing for architectural elements that may exceed base height where appropriate.
2. Architectural elements may be usable space (floor area) and should be limited to a percentage of overall floor area. This will promote different roof heights, allow view corridors to occur, and discourage large, solid walls, particularly on wide properties.



The building at Foothill and Pennsylvania features a variety of roof heights and forms, from gabled ends, hipped roofs, flat parapets and a circular rotunda.

Design and Detailing of buildings is paramount to a quality environment. Detailing, choice of materials, etc. should reinforce the overall project design. Architectural design elements, details and materials should be consistent throughout a project, recognizing that a building is 3-dimensional and must be well designed on all sides.

I. Overall Design and Detailing

1. Encourage a variety of architectural styles.
2. Continue to allow the street to have an eclectic feel.
3. Do not prescribe building location, style, open space, etc.
4. New development should have greater architectural interest than existing buildings; a variety of shapes and forms and variation in roof height and form is important.
5. Surface detailing should not serve as a substitute for well integrated and distinctive massing.
6. Provide effective transitions between commercial and residential zones. This could be in the form of well-designed building envelopes and/or providing sufficient landscaping as a buffer.
7. The ground floor of all buildings on Foothill Boulevard should be well crafted, using quality materials.
8. Elevations that face the street should be open and transparent toward the street, even if set back on the site.



Entries should be evident while well integrated into overall building.



The street façade and especially the ground floor of all buildings should be well-crafted, using quality materials.

J. Entryways

1. Entry design should be evident while well integrated into overall building.

K. Windows

1. Utilize aluminum or commercial quality storefront windows for all commercial buildings.

L. Finish Materials

1. Utilize quality materials throughout the development.
2. Use a change in materials to emphasize design features.

M. Wall Thickness

1. Window design should be appropriate to the architectural style.
2. For traditional style buildings, provide sufficient wall thickness to allow windows to be recessed.
3. For modern or contemporary buildings, flush windows are also appropriate.

N. Color

1. Natural colors should be used for buildings and structures along Foothill Boulevard.
2. Use of warmer tones rather than cool colors is encouraged.

O. Awnings

1. Awnings may be used to provide shade and identify entries.
2. Canvas awnings are appropriate, but other materials are also encouraged, depending on the architecture and details of the building.

P. Paving Materials

1. Decorative paving should be used for walkways as well as outdoor areas. Permeable paving is encouraged.

4.2b Foothill Boulevard “Main Street”

4.2b.1 Vision

This one-block stretch of Foothill Boulevard, from Lauderdale to Dunsmore Avenues, features a variety of intimate-scale retail, restaurant and service uses located within traditional storefronts. Originally subdivided with the Highway Highlands neighborhood to the south, this section of Foothill Boulevard is uniquely characterized by steep, shallow, narrow lots with limited parking and curb cuts. Structures in this block sit at or near the property line, creating a “building street edge” on both sides of the street. This “Main Street” urban form encourages neighborhood-serving retail business and similar pedestrian-oriented activities.



The one-block **Foothill Boulevard Main Street** commercial district should feature a variety of retail, restaurant and service uses. Building design in this area should pay specific attention to the pedestrian with use of entryways, windows, details and finish materials. Outdoor dining is encouraged here.

Streetscape improvements should include wide sidewalks, curb extensions, and consistent landscaping to increase visual appeal and pedestrian comfort. Complete street principles should be used to accommodate all users, from pedestrians, cyclists, buses, automobiles, and delivery trucks. Such improvements work together to develop a unique commercial district.



Landscaping, awnings and outdoor dining provide a pedestrian-oriented environment



4.2b.2 Public Improvements

The Foothill Boulevard Main Street Corridor is a Pedestrian Priority Area within a street that accommodates a variety of users. This corridor is distinguished from the rest of Foothill Boulevard with its small storefronts and blocks, as well as its proximity to community amenities including parks and schools. Mobility improvements on Foothill Boulevard “Main Street” should focus on pedestrian safety and traffic calming measures recommended in the City’s Safe and Healthy Streets Plan. Improvements will allow all modes to easily access and enjoy the amenities offered within this walkable area. While it is anticipated that the street section will be transformed to allow for these improvements, it is recommended that loss of on-street parking be minimized and existing auto and bicycle travel lanes maintained.

A. Streetscape, Landscape and Intersection Improvements for all Users

1. Consider the following enhancements within the right-of-way: (a) curb extensions with parkway landscaping and widened sidewalks, (b) dedicated travel lanes for buses, (c) street furniture amenities such as benches, pedestrian lighting and trash cans, (d) median refuges, (e) signalized mid-block pedestrian crossings.
2. Provide adequate ADA clearance for all street and mobility infrastructure improvements.
3. Consider connecting infrastructure improvements associated with the Safe Routes to School Program to Foothill Boulevard, including improvements at Dunsmore Elementary School.



Widened sidewalks, pedestrian lighting and a consistent planting of street trees all are supported in creating a more vibrant, pedestrian friendly Foothill Boulevard.



Drainage swales should be considered where possible along existing parkways.



Installing mid-block crosswalks along the Foothill Boulevard Main Street Corridor are supported to improve the shopping experience for pedestrians.

B. Improvements to the Primary Bikeway Network

1. Maintain the existing Class 2 dedicated bike lane on Foothill Boulevard.
2. Provide conveniently located bicycle parking, including bicycle corrals.
3. Support enhanced bicycle infrastructure as supported in Chapter 5 of the Safe and Healthy Streets Plan on streets identified as Primary Bikeways, including Foothill Boulevard and Dunsmore Avenue.



Transit enhancements including queue jumps, dedicated deceleration and acceleration lanes and bus stop shelters should be encouraged along Foothill Boulevard where possible.

C. Improvements to the Primary Transit Street Network

1. Maintain existing transit service on Foothill Boulevard; consider opportunities for providing additional service when feasible.
2. Consider relocating all bus stops after intersections or provide queue jumps for bus stops located before intersections for enhanced mobility.
3. Enhance bus stop amenities where possible, including shelters and/or shaded waiting areas, landscaping, benches and trash cans.



Enhanced crosswalks for bicyclists and pedestrians are supported.



Bicycle parking is encouraged and should be located conveniently next to shops and dining.



The existing Class 2 dedicated bicycle lane should be maintained along the Foothill Boulevard Main Street Corridor.

D. Improvements to Parking Management and Resources

1. Consider lifting existing time restrictions for on-street parking.
2. Encourage consolidation of driveways to maximize the amount of available on-street parking. Support reciprocal access agreements to aid in implementation of driveway reductions.

E. Public Utilities

1. Pursue funding sources, excluding assessment districts, to underground overhead utilities.
2. Provide drainage improvements in cooperation with the Los Angeles Dept of Public Works and NPDES.
3. Support use of purple pipe water for landscaping where available.

4.2b.3 Design Guidelines

In addition to the policies noted in Chapter 3, and the Vision for the future listed above, the following design guidelines are specific to the Main Street portion of Foothill Boulevard. As needed, these may be supplemented by the Comprehensive Design Guidelines. The design guidelines are intended to convey overall best practices. However, conditions vary from site to site, and there may be a more appropriate solution that is not included in the guidelines. *Innovative design solutions that are consistent with the spirit of the community vision will be considered and even encouraged.*

Site Planning involves a careful analysis of the opportunities and constraints of the site, including existing features such as mature trees, topography, and drainage patterns. The components of site development extend beyond building placement and configuration, including surrounding uses, retaining walls, landscape design, hardscape considerations, and parking.

A. Building Location

1. Locate all new development at or near the front property line.

B. Usable Open Spaces

1. Due to shallow lots and a strong street edge, there is less opportunity for usable outdoor space.
2. Where buildings have more than 100 linear feet of frontage, usable open space should be incorporated into the design to break up the building massing as viewed from the street.



Buildings at property line, with wide sidewalks and landscaping create a pedestrian-oriented street

C. Parking

1. Locate parking behind buildings.
2. Use of permeable paving, swales, and other techniques for storm water percolations is highly encouraged.

D. Landscaping

1. Maximize landscaping if the building is not located at the front property line.
2. Provide landscape design complementary to site design in all open spaces on the site.
3. Provide pedestrian amenities, including landscaping and seating along the sidewalk if the building is not located at the property line.

E. Retaining Walls

1. Provide for stepped retaining walls and/or minimize the use of retaining walls to alter grades.

Mass and Scale – New projects should fit well with surrounding building fabric. While new projects need not copy existing development, mass and scale should respect adjacent building context.

F. Relate Buildings to Existing Context

1. Relate new buildings (even if larger than existing context) to existing adjacent buildings through use of proportion, transition, or other design features.
2. Typical properties along the street vary from 50 to 100 feet. Building massing and articulation should reflect the development pattern of the neighborhood.
3. To provide appropriate massing with surrounding residences and a human scale, long, continuous segments of building walls facing a public street should be avoided.



Although the buildings vary in size, they appear to be similarly scaled



A variety of building heights and styles can relate well together, and create an eclectic and appealing streetscape.



Building massing should reflect the development pattern and heights should follow existing topography.

G. Relate Buildings to Existing Topography

1. Building heights should follow existing topography.

H. Roof Forms

1. Encourage buildings with varying roof heights, allowing for architectural elements where appropriate.
2. Roof forms should be consistent with the building design and facilitate mechanical screening.

Design and Detailing of the buildings is paramount to a quality environment. Detailing and choice of materials should reinforce the overall project design. Architectural design elements, details and materials should be consistent throughout a project, recognizing that a building is 3-dimensional and must be well designed on all sides.

I. Overall Design and Detailing – Storefront Character

1. Develop a more pedestrian-oriented commercial center for this section of Foothill Boulevard by providing high-quality design and materials for the ground floor storefront design.
2. Encourage a variety of architectural styles to maintain the eclectic nature of the street.
3. Continue façade improvements and upgrades.
4. Storefronts should maximize openness to the street with a goal of providing 65% transparency at the ground floor.

J. Entryways

1. Face building entrances and openings onto the sidewalk to promote pedestrian activity.
2. Consider highlighting building entries for visual interest and to provide a sense of arrival to the structure.
3. Provide maximum transparency (windows, pedestrian entrances) on first floor facades, with the objective to obtain 65% transparency in this pedestrian area.



Provide high-quality design and materials for the ground floor storefront design



Recessed storefronts and canopy design provide quality detailing

K. Windows

1. Face display windows toward the sidewalk to create a spatial rhythm that heightens the interest at the pedestrian level.
2. Design windows to coordinate with the architectural design of the building:
3. Use of opaque and reflective glass surfaces is discouraged.
4. Use of “security bars” is discouraged, especially along the street front.

L. Finish Materials

1. Create an active street-front environment that provides a variety of color, textures and variations in building plane.

M. Wall Thickness

1. Expression of wall thickness can be achieved by providing recessed windows and entries to exaggerate wall thickness.

N. Color

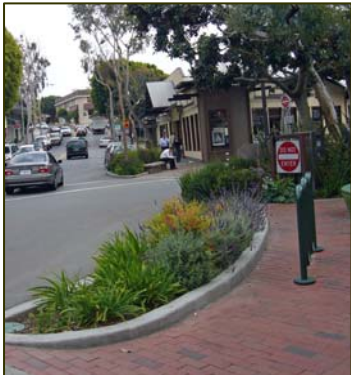
1. Use colors complementary with adjacent structures and natural environment (earth tones are encouraged).
2. Use of the following colors/materials is discouraged: Highly reflective materials and colors, especially those that produce glare, and large expanses of dark colored surfaces.

O. Awnings

1. If awnings are proposed, they should coordinate with the building, generally avoiding long and continuous treatments.

P. Paving Materials

1. Use of decorative paving treatments is encouraged at building entrances, walkways and at automobile and pedestrian contact zones.
2. Keep paving patterns simple.



Decorative paving is encouraged to highlight building and site features



Create an active street-front with opportunities for outdoor dining, awnings, and a variety of color and materials



Recessed windows with sills and stone base provide quality details

4.3a Verdugo City “Gateways” and “Village Center” Districts

4.3a.1 Vision

Verdugo City features a mixture of single- and multi-family residential areas served by commercial districts of neighborhood shopping, services, restaurants, professional offices, and community services west of the Montrose Shopping Park from Orangedale Avenue to Ramsdell Avenue. These uses will continue to function together as one community, but with a new visual identity.





Potential for signage and streetscape improvements to provide a sense of identity

Identifiable **Gateways** into Verdugo City should be created at Pennsylvania and La Crescenta Avenues with streetscape improvements and well-designed buildings. These small neighborhood shopping centers are encouraged to enhance their properties with additional landscaping, façade improvements or new development to provide a more visually pleasant experience for all users, creating an environment where existing and new businesses thrive. The intent is to create inviting entry points as one enters the City of Glendale from the Interstate 210 while providing food and convenience services to the surrounding neighborhoods.

Once a vibrant community center that began in 1925 at the intersection of Honolulu and La Crescenta Avenues, Verdugo City will regain its identity with a more vital **Village Center**. The area on Honolulu Avenue from La Crescenta to Ramsdell Avenues should be revitalized with small-scale neighborhood-serving retail and businesses, such as markets, coffee shops restaurants and realtors. The streetscape in this area should be enhanced with street trees, curb extensions and other pedestrian improvements. The Verdugo City Post Office will remain open and the historic La Crescenta Women’s Club and American Legion Hall Post 288 will continue to provide cultural activities and gathering places. Future development at the Rockhaven site will be appropriate within the surrounding context and contribute to the revitalization of Verdugo City’s Village Center.



“Gateway” and corner sites can be enhanced to provide stronger positive identity



La Crescenta Women’s Club reflects the rich cultural history of Verdugo City. These resources should be celebrated.

North Glendale Community Plan - City of Glendale, CA
Adopted Nov 29, 2011



Kenneth Village represents the type of Village Center anticipated for Verdugo City.

4.3a.2 Public Improvements

The Verdugo City Village Center on Honolulu Avenue is designated as a Pedestrian Priority Area between Ramsdell and La Crescenta Avenues. In addition, Honolulu Avenue’s designation as a Priority Bikeway allows for mobility improvements that can enhance the access and enjoyment of this neighborhood mixed-use center, patronized mainly by local residents. Mobility improvements proposed for the Verdugo City Village Center are focused on implementing a variety of traffic calming measures as supported in the Safe and Healthy Streets Plan to creating a safe, attractive and walkable neighborhood center.

A. Streetscape, landscape and intersection improvements for all users

1. Consider one or more of the following enhancements within the right-of-way: (a) curb extensions with parkway landscaping and widened sidewalks, (b) landscaping with drainage swales, (c) street furniture amenities such as benches, trash cans and pedestrian lighting, (d) median refuges, (e) signalized mid-block pedestrian crossings.
2. Provide adequate ADA clearance for all street and mobility infrastructure improvements.
3. Improve the existing signalized pedestrian crossings, including adding amenities such as curb extensions, zebra striping and median refuges.
4. Consider connecting infrastructure improvements associated with the Safe Routes to School Program to Honolulu Avenue, including improvements at John C. Fremont Elementary School.
5. Coordinate street tree planting, sidewalk widening and replacement trees with any future development site or street improvement project along Honolulu Avenue.



Safe Routes to School enhancements including zebra striped crosswalks and sharrows help promote bicycling and walking.



Pedestrian amenities at crosswalks such as curb extensions and median refuges will enhance safety for all users.



Widened sidewalks, landscaped parkways and curb extensions create a safe, attractive pedestrian-friendly environment.

B. Improvements to the Primary Bikeway Network

1. Consider traffic calming measures, including traffic circles, to allow for more efficient and safer travel on streets intersecting Honolulu Avenue within the Verdugo City Village Center.
2. Implement the Bikeway Master Plan Class 2 dedicated bike lane improvements proposed for Honolulu Avenue.
3. Implement the Bikeway Master Plan Class 2 dedicated bike lane improvements proposed for La Crescenta Avenue.
4. Provide conveniently located bicycle parking, including bicycle corrals.
5. Support enhanced bicycle infrastructure as supported in Chapter 5 of the Safe and Healthy Streets Plan on streets identified as Primary Bikeways, including Honolulu Avenue and La Crescenta Avenue.

C. Improvements to the Primary Transit Street Network

1. Maintain existing transit service on La Crescenta and Honolulu Avenue; consider opportunities for providing additional service when feasible, specifically on Honolulu Avenue west of La Crescenta Avenue.
2. Consider relocating all bus stops after intersections or provide queue jumps for bus stops located before intersections for enhanced mobility.
3. Enhance bus stop amenities where possible, including shelters and/or shaded waiting areas, landscaping, benches and trash cans.

D. Improvements to Parking Management and Resources

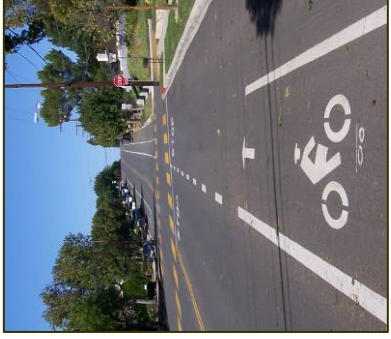
1. Encourage consolidation of driveways to maximize the amount of available on-street parking. Support reciprocal access agreements to aid in implementation of driveway reductions.



Amenities such as shelters, benches and trash cans are supported in the Verdugo City Village Center.



Wayfinding to destinations along primary bikeways is supported.



Establishing dedicated bike lanes on Honolulu and La Crescenta Avenues will help enhance North Glendale's bicycle network.

4.3a.3 Design Guidelines

In addition to the policies noted in Chapter 3, and the Vision for the future listed above, the following design guidelines are specific to the “Gateways” and “Village Center” of Verdugo City. As needed, these may be supplemented by the Comprehensive Design Guidelines. The design guidelines are intended to convey overall best practices. However, conditions vary from site to site, and there may be a more appropriate solution that is not included in the guidelines. *Innovative design solutions that are consistent with the spirit of the community vision will be considered and even encouraged.*

Site Planning involves a careful analysis of the opportunities and constraints of the site, including existing features such as mature trees, topography, and drainage patterns. The components of site development extend beyond building placement and configuration, including surrounding uses, retaining walls, landscape design, hardscape considerations, and parking.

A. Building Location

1. The Verdugo City Village Center has a variety of building types with varied relationships to the street. Wherever possible, strengthen the street edge with new development placed at the street, or additional landscaping. Locate all new development at or near the front property line.
2. Outdoor dining adjacent to the sidewalk is encouraged.
3. For sidewalk dining that may encroach into the Public Right-of-Way, and an encroachment permit must be obtained from the Public Works Department.



Enliven the street edge with development placed at the street edge, and landscaping



Maximize the amount of landscaping on the site, especially close to the street.

B. Usable Open Spaces

1. New development is encouraged to create site plans that incorporate outdoor pedestrian spaces and courtyards.
2. Outdoor areas should be integrated into the site design of new developments, surrounding buildings and existing open spaces.
3. Outdoor areas should be visible and connected to the street.
4. Where buildings are greater than 100 lineal feet of frontage, usable open space should be incorporated into the design to break up the building massing as viewed from the street.

C. Landscaping

1. Maximize the amount of landscaping on the site, especially close to the street.
2. Provide landscape design complementary to overall site design in all open spaces on the site

D. Screening

1. Locating and screening mechanical equipment is a particular challenge in this area.
2. Mechanical equipment should be placed out of public view. If equipment is located on the roof, it should be fully screened by a parapet or other method integral to the overall roof and building design.



New development is encouraged to develop outdoor pedestrian spaces, including sidewalk dining.

Mass and Scale – New projects should fit well with surrounding building fabric. While new projects need not copy existing development, mass and scale should respect adjacent building context.

E. Relate Buildings to Existing Context

1. Relate new buildings (even if larger than existing context) to existing adjacent buildings through use of proportion, transition, or other design features.
2. There are a variety of building types and styles in this area. Building design is encouraged to have a strong architectural idea. Boxy and/or monumental massing schemes are discouraged.
3. Typical development patterns along the street vary from 50 to 100 feet. Building massing and articulation should reflect the development pattern of the neighborhood. To provide appropriate massing with surrounding residences and a human scale, long, continuous segments of building walls facing the public street should be avoided.



There are a variety of building types and styles in this area.

Design and Detailing of buildings is paramount to a quality environment. Detailing and choice of materials should reinforce the overall project design. Architectural design elements, details and materials should be consistent throughout a project, recognizing that a building is 3-dimensional and must be well designed on all sides.

F. Entryways

1. Face building entrances and openings onto the sidewalk to promote pedestrian activity. Recessed building entries provide visual interest and a sense of arrival to the structure.
2. Provide maximum transparency (windows, pedestrian entrances) on first floor facades, with the objective to obtain 65% transparency in this pedestrian area.



Face display windows toward the sidewalk and provide maximum transparency.



Create active street front environment that provides color texture and variation in building plane

G. Windows

1. Face display windows toward the sidewalk to heighten the interest at the pedestrian level.
2. Design windows to coordinate with the architectural design of the building:
3. Use of opaque and reflective glass surfaces is discouraged.
4. Use of “security bars” is discouraged, especially along the street front.

H. Finish Materials

1. Create an active street front environment that provides color, textures and variations in building plane in support of the overall building designs.
2. Use of exposed concrete masonry units and split faced concrete masonry units is discouraged as a primary building material.

I. Roof Forms

1. Roof forms should be consistent with the building design style and facilitate mechanical screening.

4.3b Verdugo City Residential Neighborhoods

4.3b.1 Vision

Verdugo City contains a mix of single- and multi-family homes, with the majority of multi-family homes located closer to public transportation and commercial activities. Both the single- and multi-family areas will be maintained, keeping the unique character of each neighborhood. There will continue to be a great deal of variation throughout the neighborhood with respect to streetscape and public improvements, landscaping, and architectural design. Because of the variety throughout the area, new additions and building designs should be tailored to the immediate context, while adhering to principles in the Comprehensive Design Guidelines.





There is variety in buildings and streetscape throughout Verdugo City



Some streets have sidewalks but not all

The **Single-Family Neighborhoods south of Honolulu**, along the north face of Verdugo Mountains, will retain its existing low-density, rural residential character. Streets without curbs, sidewalks or street lights and mature landscaping will remain unless otherwise requested by the residents. Due to the variation in context from block to block, new additions and building designs should be designed to fit into the immediate context, while adhering to principles in the Comprehensive Design Guidelines.

The **Single-Family Neighborhoods south of Honolulu Avenue** will also retain its existing low-density residential character, but also allow for new multi-family development. The rural character will be maintained with most streets without curbs, sidewalks and lights, unless otherwise requested by residents. Garage locations vary based on site configuration. The variety of architectural styles provides interest and vitality to the neighborhood, and new houses or remodels fit into the existing neighborhood. Extensive mature landscaping, with many oak trees, should be maintained.

Multi-Family Neighborhoods in Verdugo City are characterized by some well-designed buildings and mature landscaping. Many of these buildings were built under older zoning regulations that allowed higher densities than the current zoning. Given the size of existing lots, the lack of vacant properties, and the fact that existing buildings are at a higher density than current zoning permits, it is unlikely that many new projects will be built or that existing buildings will be replaced with larger projects. Nonetheless, new multi-family buildings should respect the scale of the neighborhood and provide a transition in mass and scale to adjacent single-family dwellings. Safe, active, pedestrian-scaled, varied multi-family housing that clearly defines and activates the public street is encouraged.



New multi-family buildings should respect the scale of the neighborhood while providing quality design



Well crafted residences and mature landscaping characterize Verdugo City

4.3b.2 Public Improvements

Many streets in these neighborhoods do not have sidewalks, parkways, lighting and/or curbs. These improvements should be made only when interest is clearly expressed by the residents.



Large mature trees are characteristic of these properties. Some streets had sidewalks, others do not.

4.3b.3 Design Guidelines

In addition to the policies noted in Chapter 3, and the Vision for the future listed above, the following design guidelines are specific to the residential neighborhoods of Verdugo City. As needed, these may be supplemented by the Comprehensive Design Guidelines.

New multi-family buildings should respect the scale of the neighborhood and provide a transition in mass and scale to adjacent single-family dwellings. Building facades should be articulated to portray a domestic scale and give identity to individual dwelling units. Parking should be located to reduce its visibility from streets and open space areas and kept outside of required setbacks. Subterranean parking and parking courts interior to a block are appropriate solutions. Also refer to Multi-Family Design Guidelines included in the Comprehensive Design Guidelines for additional guidance for any multi-family development.

The design guidelines are intended to convey overall best practices. However, conditions vary from site to site, and there may be a more appropriate solution that is not included in the guidelines. *Innovative design solutions that are consistent with the spirit of the community vision will be considered and even encouraged.*

Site Planning involves a careful analysis of the opportunities and constraints of the site, including existing features such as mature trees, topography, and drainage patterns. The components of site development extend beyond building placement and configuration, including surrounding uses, retaining walls, landscape design, hardscape considerations, and parking.

A. Building Location

1. When there is a clearly distinguishable neighborhood pattern, new development should respect the established pattern with building location, pedestrian and vehicular access. When the development pattern varies, new development should be designed to fit into the immediate context.

B. Yards and Usable Open Space

1. Front yards in this area are open to the street, with the exception of some sloping sites which require retaining walls.

C. Garage Location and Driveways

1. Much existing development has parking and garages directly facing the street. However, the garage may face away from the street, toward the interior lot line.

D. Landscape Design (Including Hardscape)

1. Large mature trees are often characteristic of these properties. Existing trees should be maintained. New landscape design should include canopy trees of substantial size to maintain and enhance this characteristic.

E. Walls and Fences

1. The front yard should maintain its open appearance toward the street. Fences may be appropriate, where permitted. Design, materials and details must meet design and zoning criteria.

F. Retaining Walls

1. Sloping properties will require retaining walls which should be composed of cobble or native rock where possible. Other natural materials may be used if necessary.



Front yards in this area are open to the street.



Use native rock for retaining walls wherever possible.

Mass and Scale – New projects should fit well with surrounding building fabric. While new projects need not copy existing development, mass and scale should respect adjacent building context.

G. Relate Buildings to Existing Context

1. Much of the existing development in the Verdugo City residential neighborhoods is single story. Second stories in these neighborhoods should be sensitive to the existing context.
2. Consider setting back a new second story from the front face of the ground floor.

H. Relate Buildings to Existing Topography

1. Sites in this area slope in one or two directions. Locate the taller portions of the buildings on the upslope portion of the site so the building profile reflects the topography in one or both directions.

I. Scale, Proportion and Monumentality

1. These neighborhoods currently consist of homes that appear relatively modest as viewed from the street, regardless of their size. Larger homes should respect the existing context and not appear monumental, especially as viewed from the street.

J. Roof Forms

1. Existing roof forms are varied and most are pitched roofs. Due to the variety of styles, there are no preferred roof forms. Flat roofs are not inappropriate and should be designed with consideration for the existing context.



Existing roof forms are varied. Due to the variety of styles, there are no preferred roof forms.



New multi-family buildings should respect the scale of the neighborhood while providing quality design



Larger homes should respect existing context and not appear monumental.

Design and Detailing of buildings is paramount to a quality environment. Detailing and choice of materials should reinforce the overall project design. Architectural design elements, details and materials should be consistent throughout a project, recognizing that a building is 3-dimensional and must be well designed on all sides.

K. Overall Design and Detailing

1. Existing residences are well crafted, typically well detailed with two or three high quality materials. New structures DO NOT need to match the design or style of existing structures, but should be equally well crafted.

L. Entryways

1. Entries should be well integrated into the overall design, open to and visible from the street, but not monumental in scale or character.

M. Finish Materials

1. Natural materials should be used wherever possible.
2. Stone walls and stone houses characteristic of this area should be retained.

N. Wall Thickness

1. For traditional architectural designs, exterior walls should be thick enough to provide a window recess of one or two inches.



Natural materials should be used wherever possible



Stone walls and stone houses should be retained wherever possible



Well crafted residences and mature landscaping characterize this area

4.4 Glenwood Oaks, Mountain Oaks, Whiting Woods and Oakmont Woods

4.4.1 Vision

These four single-family hillside residential neighborhoods are located on the northeast face of the Verdugo Mountains and are notable for their remote, rural feel. They are grouped together because of their low density and similar characteristics such as plant types, topography and climate. Although these neighborhoods share many similarities, they were developed during different time periods, and each has unique characteristics. These existing low-density residential neighborhoods will be maintained, keeping the unique character of each neighborhood, although no significant development of adjacent hillsides is expected. New homes and additions should be sensitive to the overall scale of the existing neighborhoods and incorporate natural materials. Existing parks and community facilities should be maintained and enhanced.



Glenwood Oaks features many Ranch-style homes with attached two-car garages. Homes are finished in variety of materials, creating visual interest. Most streets are lined with liquid amber/sweet gum trees, and stands of oak trees. New homes and additions should be sensitive in mass and scale to the neighborhood context, but building style may vary.

Mountain Oaks' rural feel is enhanced by its physical isolation, extensive natural open space, stands of oak and sycamore trees and a seasonal creek. It features mostly vacant parcels, with 12 modest-sized homes scattered along the mountainside. A bridge over the wash leads to narrow, dirt and paved single-lane private roads, without curbs, sidewalks, or street lights. Some additions may be allowed and the mountainous, undeveloped character should be maintained.



Whiting Woods is notable for its extensive stands of oaks and sycamores, and seasonal creek.



Glenwood Oaks features mature landscaping and well-crafted houses.



Houses in Mountain Oaks are built with natural materials in secluded area

Whiting Woods is notable for its extensive stands of oaks and sycamores, and seasonal creek. The neighborhood features sprawling homes with horizontal massing that are nestled with the trees and topography. It includes notable works by architects John Lautner, Richard Neutra, and Clair Earl. Parcels of varying sizes are arrayed across a small network of streets that follow the contours of the mountain. New additions and developments should relate well to the low-slung horizontal character of the neighborhood.

Oakmont Woods is notable for well-crafted homes with low-slung horizontal massing. Houses are an eclectic mix providing interest and charm to the neighborhood. Parcels of similar size are arrayed across a small network of streets that follow the contours of the mountain. The streets in this area are wider than in other similar neighborhoods, and have mature landscaping with a glimpse of the mountains beyond. New homes and additions should relate well to the mass and scale of the surrounding homes, especially the horizontal massing.

4.4.2 Public Improvements

Many streets in these hillside neighborhoods do not have sidewalks, parkways, lighting and/or curbs. These improvements should be made only when interest is clearly expressed by the residents.

Access to these hillside neighborhoods is limited due to constraints of topography, land ownership and drainage facilities. Improving neighborhood access to open space and secondary emergency access may be accomplished through trail development parallel to the Verdugo Wash.



These four neighborhoods are accessible only by bridges over the Verdugo Wash



Creekbed on Whiting Woods Road and other natural features make area unique

4.4.3 Design Guidelines

In addition to the policies noted in Chapter 3 and the Vision for the future listed above, the following design guidelines are specific to Glenwood Oaks, Mountain Oaks, Whiting Woods and Oakmont Woods. As needed, these may be supplemented by the Comprehensive Design Guidelines. The design guidelines are intended to convey overall best practices. However, conditions vary from site to site, and there may be a more appropriate solution that is not included in the guidelines. *Innovative design solutions that are consistent with the spirit of the community vision will be considered and even encouraged.*

Site Planning involves a careful analysis of the opportunities and constraints of the site, including existing features such as mature trees, topography, and drainage patterns. The components of site development extend beyond building placement and configuration, including surrounding uses, retaining walls, landscape design, hardscape considerations, and parking.

A. Building Location

1. New development should make every effort to respect the established pattern with building location, pedestrian and vehicular access.
2. The presence of blueline streams in this area may limit the location of new buildings and additions.
3. Secondary ridgelines may limit the location of new buildings and additions.

B. Yards and Usable Open Space

1. Lots are varied, and landscaping is prominent throughout. Front yards are mostly open, with houses fully visible to the street.

C. Garage Location and Driveways

1. Due to the variety of lot sizes, shapes and topography, there is no regularity regarding garage location and driveways. Effort should be made to face the garage away from the street wherever possible, as the garage door is not the most attractive element of the house.

D. Landscape Design (Including Hardscape)

1. Large mature trees are often characteristic of these properties. Every attempt should be made to maintain existing trees. New landscape design should include canopy trees, including indigenous trees, that will grow to a substantial size to maintain and enhance this characteristic. However, due to wild land fire hazard in these areas, consideration of fuel modification is necessary.

E. Walls and Fences

1. Fences and walls in the front yard are discouraged and should be avoided. The front yard should maintain its open appearance toward the street.

F. Retaining Walls

1. Sloping properties may require retaining walls. Retaining walls should be used as little as possible to modify the terrain. Where necessary, retaining walls are encouraged to be composed of natural materials.



Natural landscape, well-crafted houses and no sidewalks give this area a unique and rural feel

Mass and Scale – New projects should fit well with surrounding building fabric. While new projects need not copy existing development, mass and scale should respect adjacent building context.

G. Relate Buildings to Existing Context

1. Much of existing development, particularly in Whiting Woods, is horizontally oriented ranch style development. New development and additions should reflect the horizontality of the street, if that is the predominant feature.
2. Two-story development in these neighborhoods must be sensitively handled. Consider setting back a new second story from the front face of the ground floor, and/or side setbacks for the second floor. On many streets, maintaining a low-scale appearance should be considered.

H. Relate Buildings to Existing Topography

1. Streets and many sites in this area are gently sloped, although slopes in this terrain vary. Wherever applies, locate the taller portions of the buildings on the upslope portion of the site, so the building profile reflects the topography in one or both directions.

I. Scale, Proportion and Monumentality

1. Homes in this area most often appear relatively modest as viewed from the street, regardless of their size. Larger homes should be sensitive to the existing context and not have a monumental appearance, especially as viewed from the street.

J. Roof Forms

1. Roof forms are typically hipped with a shallow pitch. While variation is valued, low pitched roofs with substantial overhangs or flat roofs are most appropriate in these neighborhoods.



Buildings are an eclectic variety that provides interest and charm to the neighborhood



Streets have mature landscaping and well-crafted homes and provide a glimpse of the mountains beyond

Design and Detailing of buildings is paramount to a quality environment. Detailing and choice of materials should reinforce the overall project design. Architectural design elements, details and materials should be consistent throughout a project, recognizing that a building is 3-dimensional and must be well designed on all sides.

K. Overall Design and Detailing

1. Existing context contains well crafted residential structures, typically well detailed with a variety (2 or 3) of high quality materials. While new structures DO NOT need to match the design or style of existing structures, new structures must be equally well crafted.

L. Entryways

1. Entries in this area should be well integrated into the overall design, open to and visible from the street. However, entries should not be monumental in scale or character.

M. Finish Materials

1. Natural materials should be used wherever possible.

N. Wall Thickness

1. For traditional architectural designs, exterior walls should be thick enough to provide a window recess of one or two inches. However, depending upon the style of the house, flush windows may be appropriate.

O. Color

1. Natural colors should be used in this area in order to blend with the woodlands, hillsides and overall natural environment.



Houses in these neighborhoods are typically well crafted homes with a low-slung horizontal appearance



4.5a Montrose Shopping Park and Sparr Heights Business District

4.5a.1 Vision

Montrose Shopping Park and Sparr Heights Business District serve a mixture of single- and multi-family residential areas. While the two are fairly close together, they are distinct and separate districts. Both business districts function well for the community and can be enhanced in the long term. There are opportunities for greater connectivity between the two districts that might benefit both; each has a unique quality that should be maintained.





Sidewalk dining, pedestrian-scaled storefronts, awnings, lush landscaping, and unique street furniture such as the clock contribute to Montrose's small-town "Mayberry" feel

Montrose Shopping Park on Honolulu Avenue is a regional "Town Center" that provides commercial services to both Crescenta Valley residents and a broader retail market area. Also known as "Historic Old Town Montrose," the district will continue to promote and capitalize upon its history, "hometown" feel, and pedestrian-friendly design that provides unity throughout the shopping street. This hometown character is further encouraged with the retention of local merchants, community festivals and a variety of shops, restaurants and cafes. Façade and parking improvements will continue to enhance the area. The "Town Center" designation will be expanded toward the west to the Montrose Library/Fire Station.

Sparr Heights Business District will continue to promote and capitalize upon its neighborhood serving commercial uses to create an increasingly vital and charming shopping district. It will continue to serve surrounding residential neighborhoods. The connections to Montrose Shopping Park will be enhanced, while each will maintain their own unique character. Because on-site and on-street parking is limited; a parking study should be conducted to identify areas for shared parking. The district will be enhanced with façade, parking and streetscape improvements, which will improve the pedestrian experience. Buildings will maintain their modest scale, providing a suitable transition into the abutting single-family residential neighborhood. The pedestrian orientation of Sparr Heights will continue to provide unity throughout the business district.



Well-crafted buildings with window displays provide a unique and attractive feel for Sparr Heights, similar to yet different from Montrose Shopping Park



Outdoor dining and awnings add to the charm of Sparr Heights

4.5a.2 Public Improvements

Montrose Shopping Park has many components that make it an ideal Pedestrian Priority Area, including wide sidewalks, ample lighting, lush landscaping and a vibrant mix of commercial businesses. As characteristic of a Town Center, Montrose businesses share a number of public parking lots. Improvements to mobility in Montrose Shopping Park are focused on maintaining existing pedestrian amenities while enhancing the experience for transit riders and bicyclists riding through or visiting the district. It is also recommended that amenities present in Montrose Shopping Park be extended west on Honolulu Avenue to Orangedale Avenue, expanding the Montrose Shopping Park experience for all users.

The Sparr Heights Business District is also designated a Pedestrian Priority Area to promote its pedestrian-friendly design and neighborhood-serving commercial uses. Mobility improvements within the Sparr Heights Business District should focus on traffic calming measures supported in the Safe and Healthy Streets Plan to better link this unique district to Montrose Shopping Park, while each will maintain their own unique identity. In conjunction with exploring potential new sources of parking, these improvements contribute to an even more vibrant, pedestrian-friendly shopping Sparr Heights.

A. Streetscape, landscape and intersection improvements for all users

1. Maintain existing pedestrian amenities, streetscape and landscape features on Honolulu Avenue.
2. Provide adequate ADA clearance for all street and mobility infrastructure improvements.
3. Consider extending pedestrian and safety amenities found in Montrose Shopping Park such as landscaped curb extensions, pedestrian street lighting, street furniture such as benches, and trash cans, from Las Palmas Avenue to Orangedale Avenue.
4. Consider one or more of the following enhancements within the right-of-way: (a) curb extensions with parkway landscaping and widened sidewalks, (b) street furniture amenities such as benches, trash cans and pedestrian lighting, (c) median refuges, (d) signalized mid-block pedestrian crossings, (e) landscaped drainage swales.
5. Support a signalized intersection for all modes of traffic at Ocean View Boulevard and Verdugo Road.
6. Consider signalizing existing mid-block pedestrian crossings.
7. Consider coordinating and connecting infrastructure improvements associated with the Safe Routes to School Program to Ocean View Boulevard and Verdugo Road, including improvements at Fremont Elementary School.



Curb extensions are supported to provide landscaping opportunities, widened sidewalks and opportunities for dining



Existing amenities in the Montrose Shopping Park, including pedestrian lighting, benches and trash cans should be maintained.

B. Improvements to the Primary Bikeway Network

1. Consider traffic calming measures, including traffic circles, to allow for more efficient and safer travel on Honolulu Avenue.
2. Implement the Bikeway Master Plan Class 3 sharrows on Honolulu Avenue.
3. Provide conveniently located bicycle parking throughout the Montrose Shopping Park on Honolulu Avenue, including bicycle corrals.
4. Support enhanced bicycle infrastructure as supported in Chapter 5 of the Safe and Healthy Streets Plan on streets identified as Primary Bikeways, including Honolulu Avenue, Verdugo Road and Ocean View Boulevard.
5. Implement the Bikeway Master Plan Class 2 dedicated bike lane for Verdugo Road.
6. Consider Class 3 sharrows on Ocean View Boulevard in conjunction with sidewalk and crosswalk improvements.
7. Provide conveniently located bicycle parking, including bicycle corrals.

C. Improvements to the Primary Transit Street Network

1. Maintain existing transit service on Honolulu Avenue and Verdugo Road; consider opportunities for providing additional service when feasible.
2. Consider an alternative source of transportation to link the Montrose Shopping Park and Sparr Heights business districts to adjacent residential neighborhoods.
3. Consider relocating all bus stops after intersections or provide queue jumps for bus stops located before intersections for enhanced mobility.
4. Enhance bus stop amenities where possible, including shelters and/or shaded waiting areas, landscaping, benches and trash cans.

D. Improvements to Parking Management and Resources

1. Support shared parking arrangements and other park-once policies to maximize parking resources.
2. Create a mechanism for adding commercial parking in the Sparr Heights Business District, including the potential for parking lots.
3. Consider conducting a parking study for Sparr Heights to determine parking needs to enable the area to attract additional businesses.
4. Encourage consolidation of driveways to maximize the amount of available on-street parking. Support reciprocal access agreements to aid in implementation of driveway reductions.



Bus stops should be enhanced to include shelters, ADA accessible sidewalks, trash cans and benches.



The installation of bike corrals should be considered to further enhance accessibility for those traveling by bicycle.



Wayfinding signs should be installed along primary bikeways to identify key destinations such as the Montrose Shopping Park and Sparr Heights.

4.5a.3 Design Guidelines

In addition to the policies noted in Chapter 3, and the Vision for the future listed above, the following design guidelines are specific to the Montrose Shopping Park and Sparr Heights Business Districts. As needed, these may be supplemented by the Comprehensive Design Guidelines. The design guidelines are intended to convey overall best practices. However, conditions vary from site to site, and there may be a more appropriate solution that is not included in the guidelines. *Innovative design solutions that are consistent with the spirit of the community vision will be considered and even encouraged.*

Site Planning involves a careful analysis of the opportunities and constraints of the site, including existing features such as mature trees, topography, and drainage patterns. The components of site development extend beyond building placement and configuration, including surrounding uses, retaining walls, landscape design, hardscape considerations, and parking.

A. Building Location

1. In Montrose Shopping Park and Sparr Heights Business District, the majority of buildings are located on the front property line, giving both districts a strong street edge and walkable sensibility. Locate all new development at or near the front property line.
2. Outdoor dining adjacent to the sidewalk is encouraged.
3. For sidewalk dining that may encroach into the Public Right-of-Way, an encroachment permit must be obtained from the Public Works Department.

B. Usable Open Spaces

1. Due to shallow lots and a strong street edge, there is less opportunity in this area for usable outdoor space in this area.
2. Where buildings are greater than 100 lineal feet of frontage, usable open space should be incorporated into the design to break up the building massing as viewed from the street.



Buildings at the property line with active street frontages provide pedestrian orientation

C. Access, Parking and Bicycles

1. Shared parking is vital to the walkability and street life of this area. Opportunities for additional shared parking should be explored.
2. Use of permeable paving, swales, and other techniques for storm water percolations is highly encouraged.
3. Provide bicycle parking, including bicycle corrals, in the overall site design.
4. Driveways should be located away from street intersections and to minimize conflict with traffic on public streets.
5. Include decorative paving materials and use of color in sidewalk and pavement areas at pedestrian/automobile contact zones.
6. Minimize pedestrian and automobile conflict by incorporating a dedicated pedestrian pathway through the parking lot area in larger projects.

D. Landscaping

1. Provide landscape design complementary to building design in all open spaces on the site.

E. Screening

1. Locating and screening mechanical equipment is a particular challenge in this area.
2. Mechanical equipment should be placed out of public view. If equipment is located on the roof, it should be fully screened by a parapet or other method integral to the overall roof and building design.

Mass and Scale – New projects should fit well with surrounding building fabric. While new projects need not copy existing development, mass and scale should respect adjacent building context.

F. Relate Buildings to Existing Context

1. Relate new buildings (even if larger than existing context) to existing adjacent buildings through use of proportion, transition, or other design features.
2. Typical development patterns along the street frontage vary from 50 to 100 feet. Building massing and articulation should reflect the development pattern of the neighborhood. To provide appropriate massing with surrounding residences and a human scale, long, continuous segments of building walls facing the public street should be avoided.



New buildings should relate to existing context through the use of proportion, transition, and other design features.



Building massing should reflect the existing development pattern.

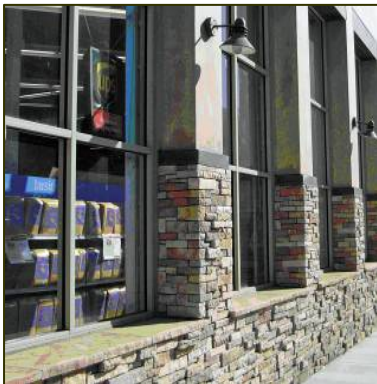
Design and Detailing of buildings is paramount to a quality environment. Detailing and choice of materials should reinforce the overall project design. Architectural design elements, details and materials should be consistent throughout a project, recognizing that a building is 3-dimensional and must be well designed on all sides.

G. Storefront Character

1. Montrose and Sparr Heights have unique characteristics in storefront design that provide particular charm to these districts. A solid base with glass above, often with recessed entries, creates a distinctive pattern that should be maintained along Honolulu Avenue and Ocean View Boulevard.
2. Continue façade improvements and upgrades while maintaining existing businesses and uses.
3. Opportunities to restore historic facades still contained beneath existing facades should be explored. New work should avoid damaging or demolishing the early facades.

H. Entryways

1. Face building entrances and openings onto the sidewalk to promote pedestrian activity.
2. Recess building entries for visual interest and to provide a sense of arrival to the structure.
3. Provide maximum transparency on first floor facades, with the objective to obtain 65% transparency in this pedestrian area.



Recessed windows with sills and stone base provide quality details



Decorative paving is encouraged to highlight building and site features



Recessed storefronts, storefront and canopy design provide quality detailing

I. Windows

1. Face display windows toward the sidewalk to heighten interest at the pedestrian level.
2. Design windows to coordinate with the architectural design of the building.
3. Use of opaque and reflective glass surfaces is discouraged.
4. Use of “security bars” is discouraged, especially along the street front.

J. Wall Thickness

1. Where appropriate to the design, expression of wall thickness can be achieved by providing recessed windows and entries.

K. Awnings

1. If awnings are proposed, they should be designed to coordinate with the architectural style of the building, generally avoiding long and continuous treatments.

L. Signage

1. Signage should be minimal in size, creative, and pedestrian oriented. The use of blade signs is encouraged.

M. Paving Materials

1. Use of decorative paving treatments is encouraged to identify pedestrian pathways on the site, at building entrances, and walkways.
2. Keep paving patterns simple and related to the architectural theme of the building.

N. Roof Forms

1. Roof forms in this area vary. However, flat roofs with parapets are predominant.
2. Roof forms should be consistent with building design and facilitate mechanical screening.



Provide maximum transparency at storefront facades.



Quality storefront design, usually with a solid base and glass above, is typical of the area.

4.5b Sparr Heights

4.5b.1 Vision

Sparr Heights consists of two distinct yet inter-connected areas – a residential neighborhood south of Montrose Shopping Park and a neighborhood commercial area along Verdugo Boulevard described in Section 4.5a. The predominantly single-family residential neighborhood has a distinctive uniform street grid, rectangular lots averaging 6500 square feet, underground utilities, distinctive street lights, sidewalks and landscaped parkways. It will retain its existing low-density character and subdivision design. The two-story multi-family housing along Downing Avenue and Glencoe Way on its southern edge will be maintained as well. The convenience, ease and enjoyment of walking, shopping and public gathering at nearby commercial districts, Lincoln Elementary School, and Sparr Heights Community Center should be promoted.





Streets in Sparr Heights are lined with mature trees, sidewalks and an eclectic mix of architectural styles

Retaining much of its original character, **Single-Family Homes in Sparr Heights** are modestly scaled, with ample front, side and back yards. Most streets typically have open front yards with houses that address the street with entries or large openings. Garages are typically detached from the house and located at the rear of the property. The neighborhood contains a mix of period architectural styles. The variety in this neighborhood is one of its strengths and is encouraged. Additions to existing homes, sensitive to the neighborhood scale, are expected.



Well-crafted homes with open front yards and landscaping are typical in Sparr Heights

The **Multi-Family Residences in Sparr Heights** should be maintained. Many of these buildings were developed under older zoning regulations that permitted higher densities than the current zoning. Given the size of existing lots, the lack of vacant properties, and the fact that existing development is higher density than what would be allowed under current zoning, it is unlikely that many new projects will be built or that existing buildings will be replaced with larger projects. Nonetheless, new multi-family buildings should respect the scale of the neighborhood and provide a transition in mass and scale to adjacent single-family dwellings.



Existing multi-family buildings are typically postwar era with courtyards

4.5b.2 Public Improvements

The Sparr Heights residential neighborhood is directly adjacent to two pedestrian priority areas, the Montrose Shopping Park and Sparr Heights Business District. It is also walking distance to the Sparr Heights Community Center, Montrose Library and Fremont Elementary School. Policies proposed for this walkable neighborhood are focused on enhancing available transit, pedestrian and bicycle amenities to promote alternative modes of transportation and traffic calming measures to limit cut-through vehicular traffic. In addition, any proposed development in the neighborhood is encouraged to provide communal and private open space to retain available green space in the neighborhood.

A. Mobility Improvements

1. Continue to utilize existing transit lines on major streets (i.e., Verdugo Boulevard) that are near the densest residential area in Sparr Heights.
2. Promote walking and bicycling by providing well-designed trails and facilities.
3. Retain or add traffic-calming measures, sidewalks with parkways (e.g., planting strips), and more attractive and functional pedestrian/bicycle facilities.
4. Avoid traffic spillover into the residential neighborhood.

B. Open Space Improvements

1. Ensure that new multi-family development provides attractive, centrally located communal open space with functional amenities. The amount of communal open space should increase with the size of a multi-family development to meet the social and recreational needs of its inhabitants.
2. Ensure that new multi-family development includes private outdoor space in conjunction with each dwelling unit. Private open space may include patios, terraces, and well sized balconies.
3. Ensure that new and existing neighborhoods contain a diverse mix of parks and open spaces that are connected by trails, bikeways, and other open space networks and are within easy walking distance of residents.
4. Maintain and support the services of Montrose Park, which is located on the border of Sparr Heights and also serves Montrose and Montecito Park.

4.5b.3 Design Guidelines

In addition to the policies noted in Chapter 3, and the Vision for the future listed above, the following design guidelines are specific to the residential neighborhoods of Sparr Heights. As needed, these may be supplemented by the Comprehensive Design Guidelines.

New multi-family buildings should respect the scale of the neighborhood and provide a transition in mass and scale to adjacent single-family dwellings. Building facades should be articulated to portray a domestic scale and give identity to individual dwelling units. Parking should be located to reduce its visibility from streets and open space areas and kept outside of required setbacks. Subterranean parking and parking courts interior to a block are appropriate solutions. Also refer to Multi-Family Design Guidelines included in the Comprehensive Design Guidelines for additional guidance for any multi-family development.

The design guidelines are intended to convey overall best practices. However, conditions vary from site to site, and there may be a more appropriate solution that is not included in the guidelines. *Innovative design solutions that are consistent with the spirit of the community vision will be considered and even encouraged.*

Site Planning involves a careful analysis of the opportunities and constraints of the site, including existing features such as mature trees, topography, and drainage patterns. The components of site development extend beyond building placement and configuration, including surrounding uses, retaining walls, landscape design, hardscape considerations, and parking.

A. Building Location

1. New development should make every effort to respect the established pattern with building location, pedestrian and vehicular access.
2. Dwellings should orient to the street, with the garage set back from the front elevation or placed to the rear of the property.

B. Yards and Usable Open Space

1. Front yards in this area are open to the street, with the exception of some sloping sites which require retaining walls.



Well-crafted homes with open front yards and landscaping are typical in Sparr Heights



Dwellings are oriented to the street, with open front yards with large mature trees.

C. Garage Location and Driveways

1. Garages are typically detached from the house and located at the rear of the property. For new homes, effort should be made to minimize the amount of paved area and to face the garage away from the street wherever possible, as the garage door is not the most attractive element of the house.
2. When garage doors are visible from the street, care should be taken to avoid repetitive placement of garage doors and reduce their visual prominence.

D. Landscape Design (Including Hardscape)

1. Large mature trees are often characteristic of these properties, and every attempt should be made to maintain existing trees.
2. New landscape design should include canopy trees that will grow to a substantial size to maintain and enhance this characteristic.
3. Hardscape should be minimized and use of permeable pavement is highly encouraged.

E. Walls and Fences

1. Fences and walls in the front yard are discouraged and should be avoided, with the front yard maintaining its open appearance toward the street.

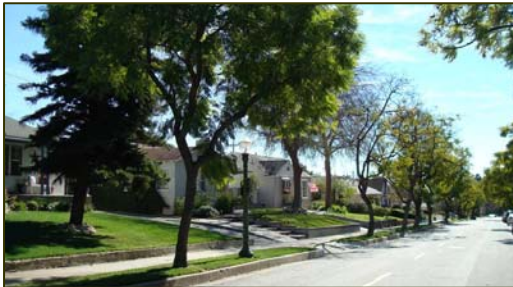
F. Retaining Walls

1. Retaining walls should be used as little as possible to modify the terrain.
2. Where necessary, retaining walls are encouraged to be composed natural materials.

Mass and Scale – New projects should fit well with surrounding building fabric. While new projects need not copy existing development, mass and scale should respect adjacent building context.

G. Relate Buildings to Existing Context

1. Excessively large dwellings that dominate the surrounding neighborhood are discouraged.
2. Dwellings should present a simple and pleasing composition that uses a common vocabulary of forms, architectural elements, and materials.
3. When façades are altered they should be articulated, or retain existing articulation, to add interest, reduce monotony, and create identity from dwelling to dwelling.
4. The size and scale of development in the Sparr Heights neighborhood appears modest as viewed from the street. New development and additions should be sensitive in mass and scale to adjacent development and the context along the street.
5. Two-story development in this neighborhood is typically modest and does not reflect the full building footprint. Consider setting back a new second story from the front face of the ground floor.



The size and scale of development is modest as viewed from the street.



Dwellings should present a simple and pleasing composition using a common vocabulary of forms.

H. Relate Buildings to Existing Topography

1. Sites in this area are generally flat or gently sloped. When building on a sloped site or street, locate the taller portions of the buildings on the upslope portion of the site so the building profile reflects the topography in one or both directions.

I. Scale, Proportion and Monumentality

1. While this neighborhood is varied, the homes may appear relatively modest as viewed from the street, regardless of their size. Larger homes should be sensitive to the existing context and not have a monumental appearance, especially as viewed from the street.

J. Roof Forms

1. Roof forms in the existing context are varied, the majority being pitched of one type or other.
2. Due to the variety of styles and forms in the area, there are no preferred roof forms.
3. Flat roofs are not inappropriate but should be designed with consideration for the existing context.

Design and Detailing of the buildings is paramount to a quality environment. Detailing and choice of materials should reinforce the overall project design. Architectural design elements, details and materials should be consistent throughout a project, recognizing that a building is 3-dimensional and must be well designed on all sides.

K. Overall Design and Detailing

1. A pleasing variety of home styles is encouraged. In particular, dwellings with identical elevations should not be placed on adjacent lots, and developments must include single-story units and/or building forms.
2. Existing context contains well-crafted residential structures, typically well detailed with a variety (2 or 3) of high quality materials. While new structures DO NOT need to match the design or style of existing structures, new structures must be equally well crafted.

L. Entryways

1. Entries in this area should be well integrated into the overall design, open to and visible from the street. However, entries should not be monumental in scale or character.



Well-crafted residential structures with a variety of materials is typical in Sparr Heights.



A pleasing variety of building styles with use of natural materials is encouraged.

M. Finish Materials

1. Natural materials should be used wherever possible.
2. Stone walls and stone houses characteristic of this area should be retained.

N. Wall Thickness

1. If a traditional architectural design is proposed, exterior walls should be thick enough to provide a window recess.

O. Color

1. Natural colors should be used in this area in order to blend with the hillside and the overall environment.

4.6 Indian Springs

4.6.1 Vision

Developed with the Verdugo Hills Hospital and professional office buildings, this area will be enhanced through strategic development. These important medical-oriented uses offer convenient professional services to the residents of North Glendale, La Crescenta, and La Canada Flintridge. The existing hospital and medical uses will be maintained and enhanced. Further development of medical uses and office buildings is encouraged, with the scale and character of new development respectful to existing context. The multi-family residential units (east of Valihi Way and west of the Glendale/2 Freeway) should be protected and enhanced. The small-scale commercial centers should be enhanced to provide a sense of identity and improved pedestrian orientation, providing greater visual connectivity for the variety of community-serving retail.





The “Indian Springs” commercial center after renovation in 2010

Indian Springs is an ideal location for a mid-size boutique hotel to serve visitors to the Crescenta Valley due to its proximity to freeways, Montrose, Verdugo Hills Hospital, Descanso Gardens and the Jet Propulsion Laboratory.

A campus-type environment for medical and associated professional uses should be created adjacent to the Verdugo Hills Hospital.

Create a pedestrian link from the Montrose Shopping Park to the movie theater complex in La Canada Flintridge.

To identify Indian Springs as a gateway to the North Glendale from La Canada Flintridge, an entry monument sign should be provided on Verdugo Road.



Verdugo Hills Hospital



The series of Ranch-style apartment buildings arrayed along Valihi Way and Stancrest Drive represents an unusually cohesive grouping of multi-family structures in the Ranch style. In scale, they are often reminiscent of single-family houses, but typically consist of duplexes and triplexes, with a few larger buildings containing up to twelve units.

4.6.2 Public Improvements

Verdugo Boulevard is the main commercial corridor of Indian Springs. It is a link in the Primary Auto Network with an access to the 210 Freeway, as well as a Primary Bikeway connecting North Glendale to Pasadena, and contains an LADOT Commuter Express stop. Mobility improvements should focus on providing a safer environment for the transit user, bicyclist and pedestrian without impeding mobility for the auto user.

A. Streetscape, landscape and intersection improvements for all users

1. Consider the following enhancements within the right-of-way: (a) curb extensions with parkway landscaping and widened sidewalks, (b) landscaping with drainage swales.
2. Provide adequate ADA clearance for all street and mobility infrastructure improvements.
3. Coordinate street tree planting, sidewalk widening and replacement trees with any future development site or street improvement project along Verdugo Road.



Landscape buffers are encouraged between buildings and the right-of-way



Parkway landscaping with drainage swales are supported along the Verdugo Boulevard right-of-way.



Widened sidewalks and street trees should be coordinated with development.

B. Improvements to the Primary Bikeway Network

1. Maintain the existing Class 2 dedicated bike lane on Verdugo Boulevard.
2. Support enhanced bicycle infrastructure as supported in Chapter 5 of the Safe and Healthy Streets Plan on streets identified as Primary Bikeways, including Verdugo Boulevard.
3. Consider bicycle signalization, crosswalk improvements or other safety enhancements where Verdugo Boulevard meets the 2 Freeway.

C. Improvements to the Primary Transit Street Network

1. Maintain existing transit service on Verdugo Boulevard; consider opportunities for providing additional service when feasible.
2. Enhance bus stop amenities where possible, including shelters and/or shaded waiting areas, landscaping, benches and trash cans.
3. Coordinate with the City of La Canada Flintridge to enhance LADOT bus stop amenities on Verdugo Boulevard, including shelters and/or shaded waiting areas, landscaping, benches and trash cans.

D. Improvements to Parking Management and Resources

1. Consider lifting existing time restrictions for on-street parking.

4.6.3 Design Guidelines

In addition to the policies noted in Chapter 3, and the Vision for the future listed above, the following design guidelines are specific to Indian Springs. As needed, these may be supplemented by the Comprehensive Design Guidelines. For guidance on Multi-Family development in Indian Springs, refer to Multi-Family Design Guidelines included in the Comprehensive Design Guidelines. The design guidelines are intended to convey overall best practices. However, conditions vary from site to site, and there may be a more appropriate solution that is not included in the guidelines. *Innovative design solutions that are consistent with the spirit of the community vision will be considered and even encouraged.*

Site Planning involves a careful analysis of the opportunities and constraints of the site, including existing features such as mature trees, topography, and drainage patterns. The components of site development extend beyond building placement and configuration, including surrounding uses, retaining walls, landscape design, hardscape considerations, and parking.

A. Building Location

1. The Indian Springs area has a variety of building types with varied relationships to the street. Wherever possible, strengthen the street edge with new development placed at the street, or with additional landscaping. Locate all new development at or near the front property line.
2. Create a pedestrian link from the Montrose Shopping Park to the movie theater complex.

B. Usable Open Spaces and Courtyards

1. New development is encouraged to create site plans that incorporate outdoor pedestrian spaces and courtyards.
2. Outdoor areas should be integrated into the site design of new developments, surrounding buildings and existing open spaces.
3. Outdoor areas should be visible and connected to the street.
4. Where buildings are greater than 100 lineal feet of frontage, usable open space should be incorporated into the design to break up the building massing as viewed from the street.



Planters and landscaping, street trees with parkway, and decorative paving enhance the pedestrian experience



Strengthen the street edge with new development placed at the street, outdoor dining or landscaping.

C. Access and Parking

1. Driveways should be located away from street intersections and to minimize conflict with traffic on public streets.
2. Minimize pedestrian and automobile conflict by incorporating a dedicated pedestrian pathway through the parking lot area in larger projects and at locations where pedestrian meets vehicular traffic.
3. Use of permeable paving, swales, and other techniques for storm water percolations is highly encouraged.

D. Landscaping

1. Maximize the amount of landscaping on the site, especially close to the street.
2. Provide landscape design complementary to site and building design in all open spaces on the site.

E. Screening

1. Mechanical equipment should be placed out of public view.
2. If equipment is located on the roof, it should be fully screened by a parapet or other method integral to the overall roof and building design.

Mass and Scale – New projects should fit well with surrounding building fabric. While new projects need not copy existing development, mass and scale should respect adjacent building context.

F. Relate Buildings to Existing Context

1. Relate new buildings (even if larger than existing context) to existing adjacent buildings through use of proportion, transition, or other design features.
2. There are a variety of building types and styles in this area. Building design is encouraged to have a strong architectural idea. Boxy and/or monumental massing schemes are discouraged.
3. Typical development patterns along the street vary from 50 to 100 feet. Building massing and articulation should reflect the development pattern of the neighborhood. To provide appropriate massing with surrounding residences and a human scale, long, continuous segments of building walls facing a public street should be avoided.



Commercial buildings should be well designed and properly scaled to the surrounding context



Indian Springs has a cohesive grouping of well-designed multi-family structures.



G. Entryways

1. Face building entrances and openings onto the sidewalk to promote pedestrian activity.
2. Recess building entries for visual interest and to provide a sense of arrival to the structure.

H. Windows

1. Face display windows toward the sidewalk to heighten visual interest at the pedestrian level.
2. Design windows to coordinate with the architectural design of the building.
3. Use of opaque and reflective glass surfaces is discouraged.
4. Use of “security bars” is discouraged, especially along the street front.

I. Wall Thickness

1. Where appropriate to the design, expression of wall thickness can be achieved by providing recessed windows and entries.

J. Awnings

1. If awnings are proposed, they should be designed to coordinate with the architectural style of the building, generally avoiding long and continuous treatments.

K. Paving Materials

1. Use of decorative paving treatments is encouraged to identify pedestrian pathways on the site, at building entrances, and walkways.
2. Keep paving patterns simple and related to the architectural theme of the building.
3. Use of permeable paving is highly encouraged.

L. Roof Forms

1. Roof forms in this area vary. However, flat roofs with parapets are predominant.
2. Roof forms should be consistent with building design and facilitate mechanical screening.

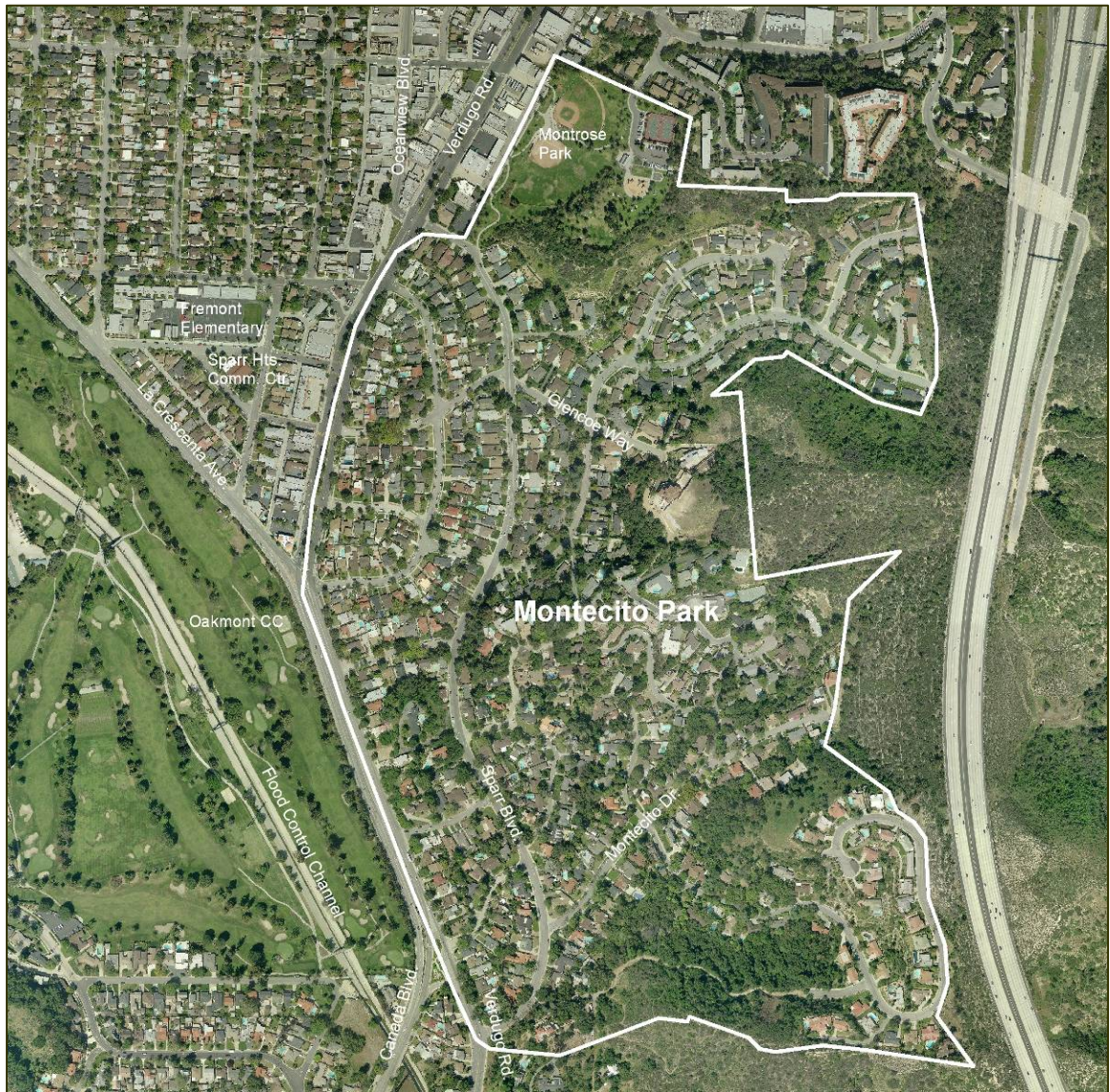


Building design is encouraged to have a strong architectural idea

4.7 Montecito Park

4.7.1 Vision

Montecito Park is walkable to several amenities in North Glendale, including Montrose Community Park and the Sparr Heights Business District. Originally part of the Sparr Heights subdivision, the neighborhood's varied topography, street layout, larger properties and later period of residential construction gives Montecito Park a unique character that is distinct and separate from the original Sparr Heights subdivision. Nestled against the hillsides along the northeastern end of Verdugo Canyon, Montecito Park will retain its existing low-density residential character.





Montecito Park's street layout is a curvilinear grid, with streets undulating uphill to reflect the hillside topography at its eastern edge. Lot sizes are varied, generally increasing in size and irregularity at higher elevations in the neighborhood. Streets along the curvilinear grid contain parkways, street lights and sidewalks, while those higher up in the hillsides are often absent these amenities. This existing pattern should remain unless otherwise requested by a majority of the residents. Landscaping in Montecito Park should remain lush, with canopies of oak and sycamore trees throughout.

The neighborhood has a varied and eclectic mix of architectural styles. Massing is varied, ranging from modest one-story homes to larger two-story homes. Additions should be sensitive to the overall scale of surrounding houses. The variety and eclectic mix of architectural styles gives the neighborhood a unique visual character and is encouraged. New houses or remodels should continue this eclectic spirit.



Well-designed monument sign and mature landscaping add to the unique identity of the area



Mature landscaping in front of horizontally oriented residential buildings is characteristic

4.7.2 Public Improvements

Many streets in this neighborhood do not have sidewalks, parkways, lighting and/or curbs. These improvements should be made only when interest is clearly expressed by the residents.

4.7.3 Design Guidelines

In addition to the policies noted in Chapter 3, and the Vision for the future listed above, the following design guidelines are specific to Montecito Park. As needed, these may be supplemented by the Comprehensive Design Guidelines. The design guidelines are intended to convey overall best practices. However, conditions vary from site to site, and there may be a more appropriate solution that is not included in the guidelines. *Innovative design solutions that are consistent with the spirit of the community vision will be considered and even encouraged.*

Site Planning involves a careful analysis of the opportunities and constraints of the site, including existing features such as mature trees, topography, and drainage patterns. The components of site development extend beyond building placement and configuration, including surrounding uses, retaining walls, landscape design, hardscape considerations, and parking.

A. Building Location

1. While lots and building placement vary throughout the neighborhood, new development should make every effort to respect the established pattern on the street with building location, pedestrian and vehicular access.

B. Yards and Usable Open Space

1. Front yards in this area are open to the street, with the exception of some sloping sites which require retaining walls.

C. Garage Location and Driveways

1. Due to the variety of lot sizes, shapes and topography, there is no regularity regarding garage location and driveways. Effort should be made to face the garage away from the street wherever possible, as the garage door is not the most attractive element of the house.



Retaining walls should be used rarely to modify the terrain. When necessary, they should be low and composed of natural materials.



D. Landscape Design (Including Hardscape)

1. Large mature trees are often characteristic of these properties. Every attempt should be made to maintain existing trees. New landscape design should include canopy trees that will grow to a substantial size to maintain and enhance this characteristic.
2. Some properties in this area interface with wild lands and consideration of fire-fuel modification is necessary.
3. Minimize use of hardscape in this area. Utilize permeable paving as much as possible.

E. Walls and Fences

1. Fences and walls in the front yard are discouraged and should be avoided. The front yard should maintain its open appearance toward the street.

F. Retaining Walls

1. Retaining walls should be used as little as possible to modify the terrain. Where necessary, retaining walls are encouraged to be composed natural materials.

Mass and Scale – New projects should fit well with surrounding building fabric. While new projects need not copy existing development, mass and scale should respect adjacent building context.

G. Relate Buildings to Existing Context

1. The size and scale of development in Montecito Park is varied, dependent upon lot size and configuration. New development and additions should be sensitive in mass and scale to adjacent development and the context along the street.
2. Two-story development in this neighborhood is typically configured of several different forms, not boxy volumes, unless a singular form is appropriate to the building style. Consider setting back a new second story from the front face of the ground floor.

H. Relate Buildings to Existing Topography

1. Sites in this area generally slope in one or two directions. Locate the taller portions of the buildings on the upslope portion of the site so the building profile reflects the topography in one or both directions.

I. Scale, Proportion and Monumentality

1. While this neighborhood is varied, the homes may appear relatively modest as viewed from the street, regardless of their size. Larger home should be sensitive to the existing context and not have a monumental appearance, especially as viewed from the street.



Homes appear modest as viewed from the street, regardless of their size.



Consider setting back a new second story from the front face of the ground floor, to prevent a massive appearance.

J. Roof Forms

1. Roof forms in the existing context are varied; the majority are pitched roofs of one type or other. Due to the variety of styles and forms in the area, there are no preferred roof forms. Flat roofs are not inappropriate, but should be designed with consideration for the existing context.

Design and Detailing of buildings is paramount to a quality environment. Detailing and choice of materials should reinforce the overall project design. Architectural design elements, details and materials should be consistent throughout a project, recognizing that a building is 3-dimensional and must be well designed on all sides.

K. Overall Design and Detailing

1. Existing context contains well-crafted residential structures, typically well detailed with a variety (2 or 3) of high-quality materials. While new structures DO NOT need to match the design or style of existing structures, new structures must be equally well crafted.

L. Entryways

1. Entries in this area should be well integrated into the overall design, open to and visible from the street. However, entries should not be monumental in scale or character.

M. Finish Materials

1. Natural materials should be used wherever possible.

N. Wall Thickness

1. If a traditional architectural design is proposed, exterior walls should be thick enough to provide a window recess.

O. Color

1. Natural colors should be used in this area in order to blend with the hillside and the overall environment.



Entries should be well-integrated into the overall design, open to and visible from the street.



Montecito Park contains well-crafted residential structures, typically with a variety of high-quality materials.

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